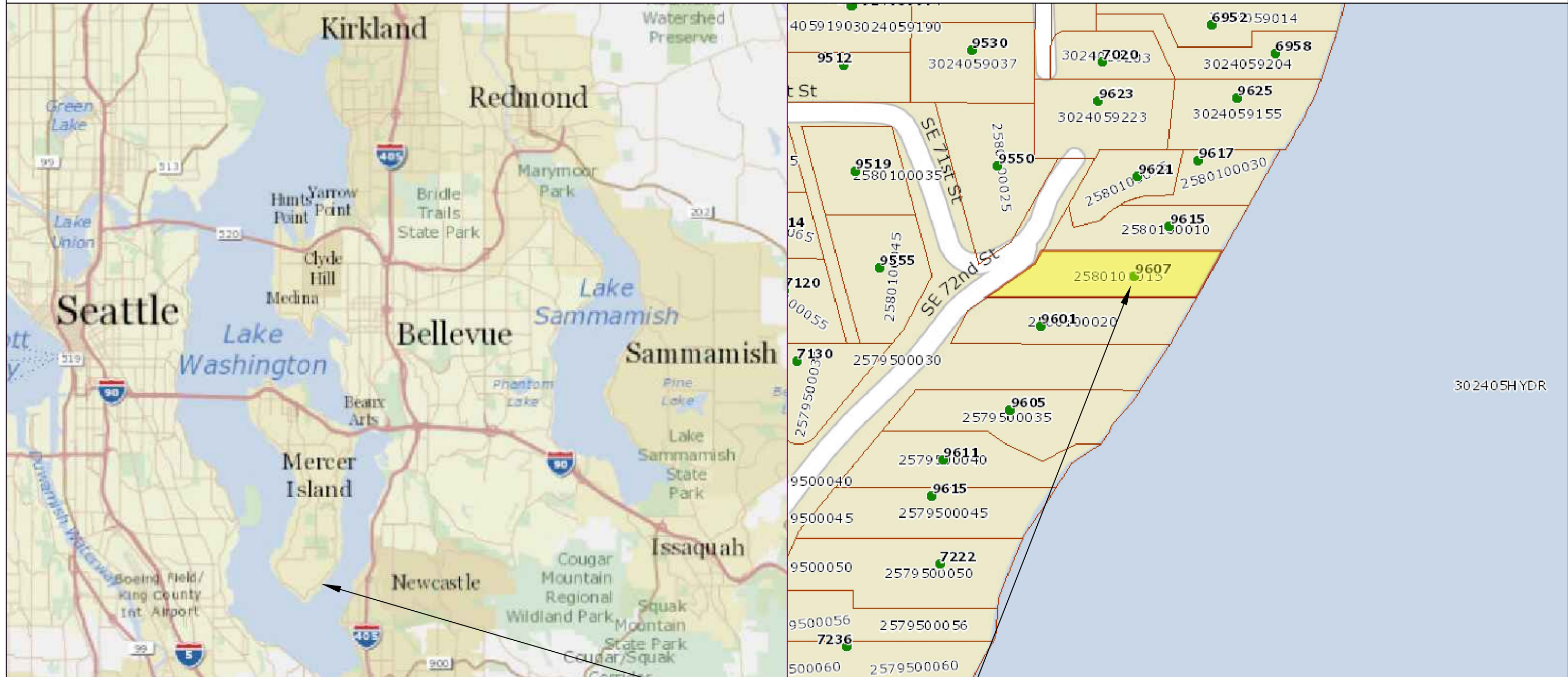


# SITE PLAN



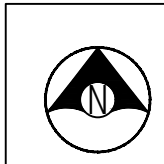
Pin: 258010-0015

Legal Description: FLOODS LAKE SIDE TRS DIV # 2 & SH LDS ADJ

Plat Block: 1  
Plat Lot: 3

LAT: 47.53866      LONG: -122.21059

**SUBJECT  
PROPERTY**



Seaborn Pile Driving  
1080 W Ewing St  
Seattle, WA 98119

Office: 206-236-1700 ext. 3  
www.seabornpiledriving.com

Scope of Work: We propose to repair (14) dock piles, repair the 57 linear feet of existing rock bulkhead, remove approximately 214 sqft of hardscape, remove (6) dock piles & repair the existing decking with grated material.

County: King County  
Location: Lake Washington

Applicant: George Residence  
9607 SE 72nd Street  
Mercer Island, WA 98040

Datum: CORPS OF ENGINEERS 1919  
NE Quarter Of Section 30, Township 24, Range 05

Adjacent Owners:  
BARTO M BRUCE+SANDRA S  
9601 SE 72ND ST 98040

GARTON KYLE JHRACHEL A  
9615 SE 71ST ST 98040

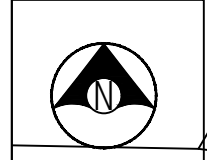
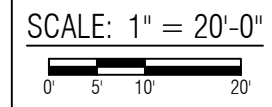
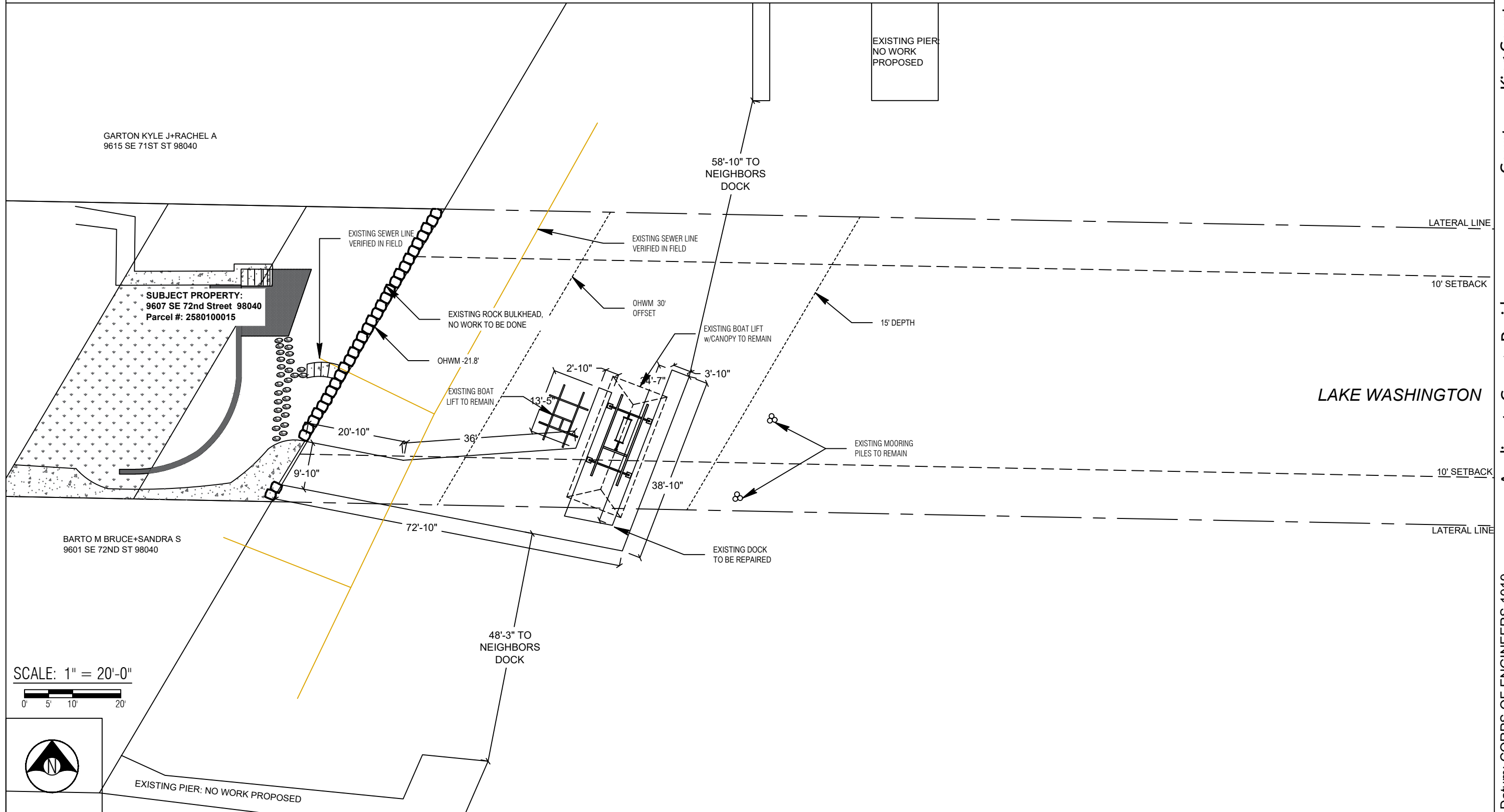
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NWS-2021-XXX  
PAGE 1 OF 14

Created: 1-24-2022  
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# EXISTING CONDITIONS

**\*\*CLEAN UP LAKE AROUND PROJECT\*\***



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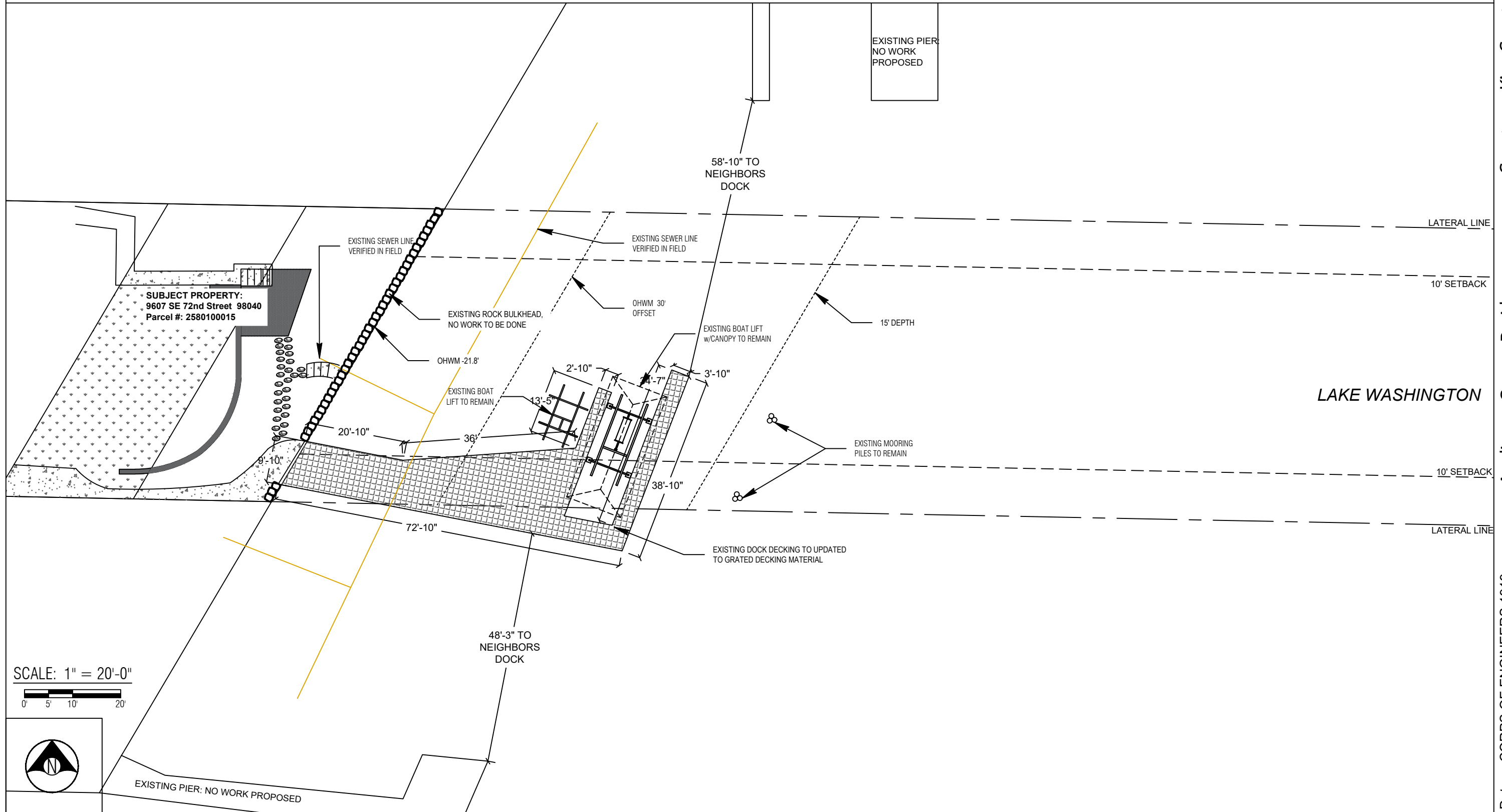
Datum: CORPS OF ENGINEERS 1919  
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 PAGE 2 OF 14

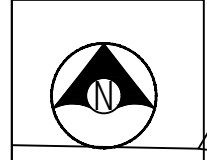
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# PROPOSED CONDITIONS

**\*\*CLEAN UP LAKE AROUND PROJECT\*\***



SCALE: 1" = 20'-0"  
 0' 5' 10' 20'



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**SHEET  
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# PIER DETAILS - EXISTING/PROPOSED

## LEGEND

- ⊕ (16) EXISTING DOCK PILES - TO BE REPAIRED
- ⊗ (6) EXISTING DOCK PILES - TO BE REMOVED
- (3) EXISTING DOCK PILES - NO WORK TO BE DONE
- (6) EXISTING DOLPHIN PILES - NO WORK TO BE DONE

Area: 965 sqft (overwater decking)  
 Area: 965 sqft (new grated decking)

\*\*Grated is a 43% light permeable material

## EXISTING

- (12) EXISTING CAPS
- (36) EXISTING PILES
- (91) EXISTING STRINGERS
- (72) EXISTING PLATES

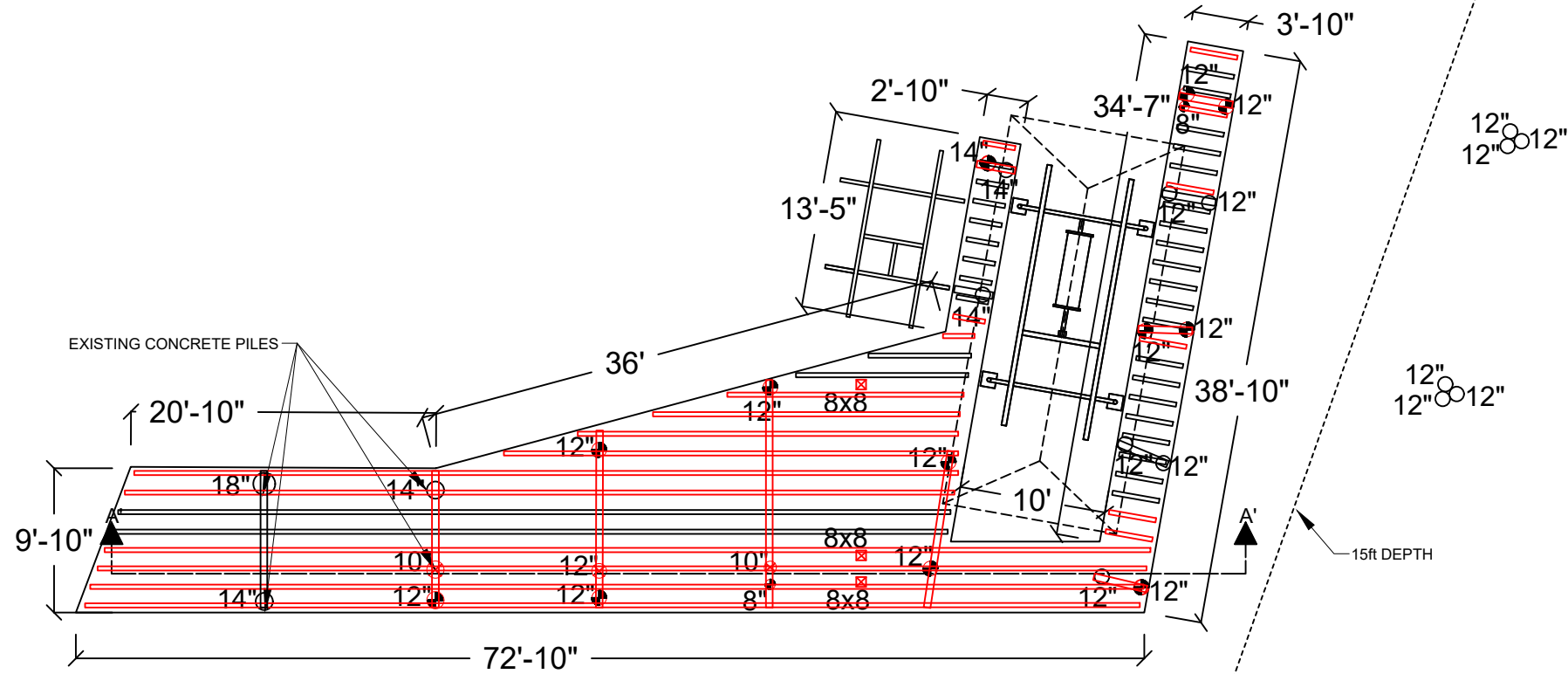
(211) TOTAL STRUCTURE ELEMENTS

## TO BE REPAIRED

- (8) EXISTING CAPS TO BE REPAIRED
- (16) EXISTING PILES TO BE REPAIRED
- (53) EXISTING STRINGERS TO BE REPAIRED
- (28) EXISTING PLATES TO BE REPAIRED

(105) TOTAL STRUCTURE ELEMENTS TO BE REPAIRED

$(105)/(211) = 50\%$



PLAN VIEW



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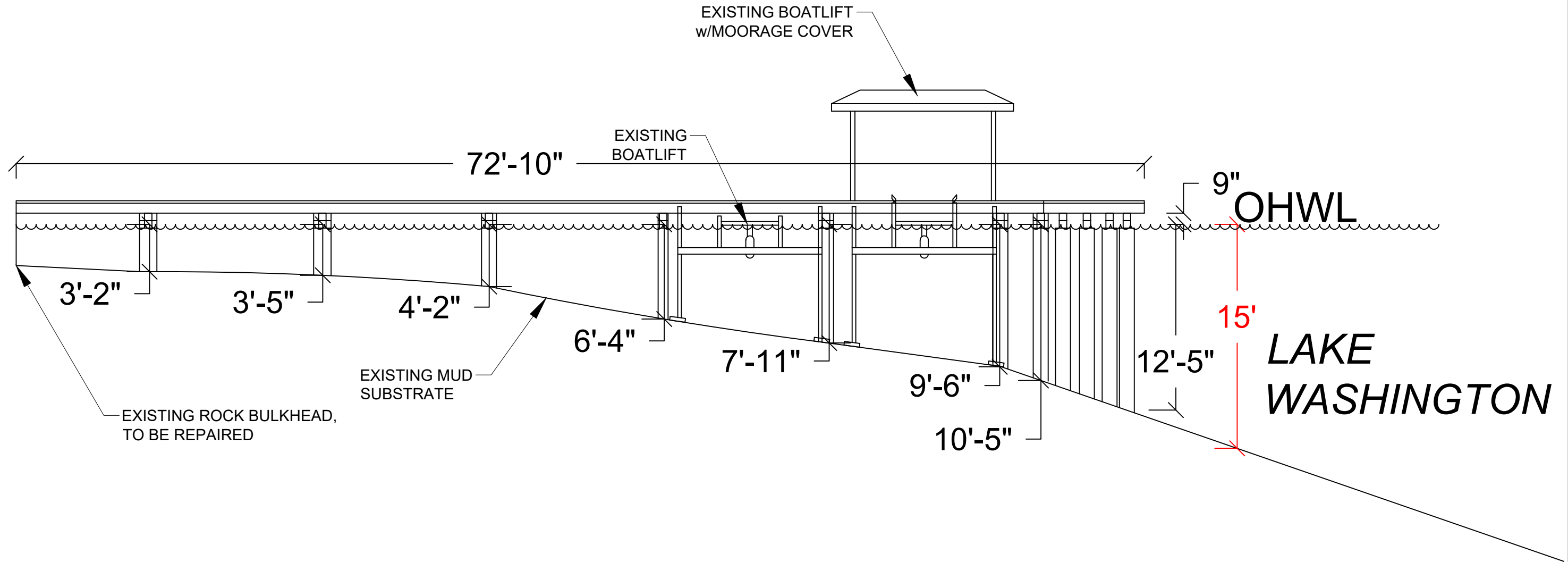
GARTON KYLE J+RACHEL A  
 9615 SE 71ST ST 98040

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**A4.0**

# PIER DETAILS EXISTING/PROPOSED



SECTION VIEW: A'-A'



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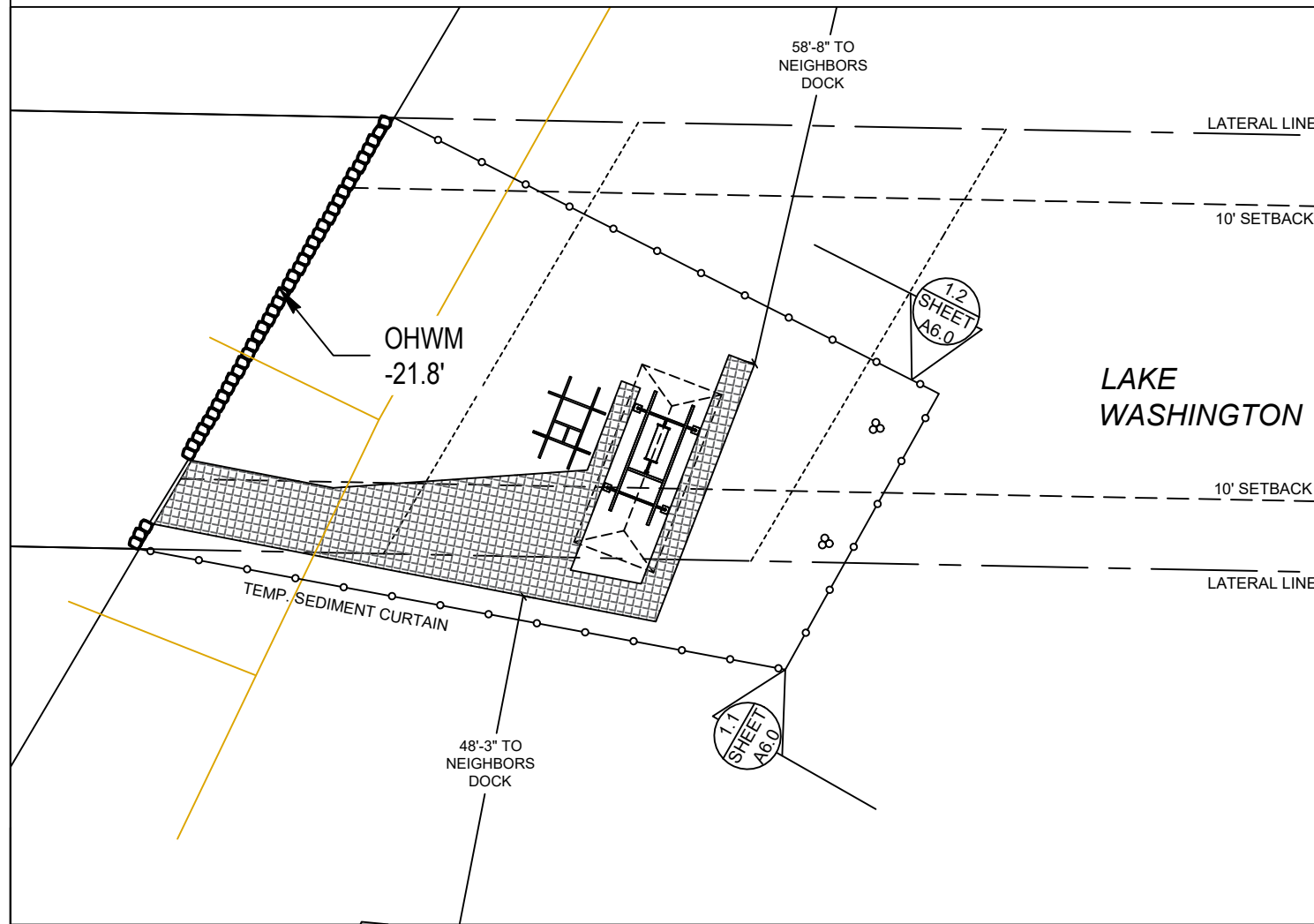
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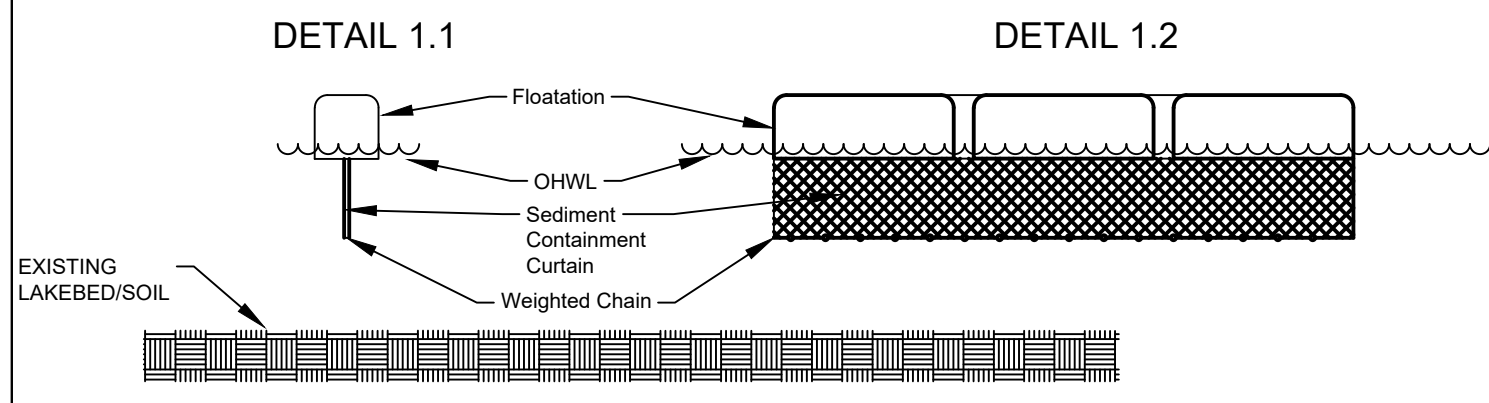
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# BMP INFORMATION



## BMP NOTES:

- A. Constant vigilance shall be kept for the presence of protected fish species during all aspects of the proposed action, particularly during in-water activities such as vessel movement, deployment of anchors & spuds, pile driving, dredging, and placement of gravels and other fill.
  1. The project manager shall designate an appropriate number of competent observers to survey the project site and adjacent areas for protected species, including the presence of fish as conditions allow.
  2. Visual surveys shall be made prior to the start of work each day, and prior to resumption of work following any break of more than an hour. Periodic additional visual surveys throughout the work day are strongly recommended.
  3. All in-water work shall be done during the in-water work window for the waterbody. Where there is a difference between the USCOE and WDFW work windows, the overlap of the two shall apply.
  4. All pile driving and extraction shall be postponed or halted when obvious aggregations or schooling of fish are observed within 50 yards of that work, and shall only begin/resume after the animals have voluntarily departed the area.
  5. When piloting vessels, vessel operators shall operate at speeds and power settings to avoid grounding vessels, and minimize substrate scour and mobilization of bottom sediments.
- B. No contamination of the marine environment shall result from project-related activities.
  1. Appropriate materials to contain and clean potential spills shall be stored and readily available at the work site and/or aboard project-related vessels.
  2. The project manager and heavy equipment operators shall perform daily pre-work equipment inspections for cleanliness and leaks. All heavy equipment operations shall be postponed or halted should a leak be detected, and shall not proceed until the leak is repaired and the equipment is cleaned.
  3. To the greatest extent practicable, utilize biodegradable oils for equipment that would be operated in or near water.
  4. Fueling of land-based vehicles and equipment shall take place at least 50 feet away from the water, preferably over an impervious surface. Fueling of vessels shall be done at approved fueling facilities.
  5. Turbidity and siltation from project-related work shall be minimized and contained through the appropriate use of erosion control practices, effective silt containment devices, and the curtailment of work during adverse weather and tidal/flow conditions.
  6. All wastes shall be collected and contained for proper disposal at approved upland disposal sites appropriate for the material(s).
  7. When removing piles and other similarly treated wood, containment booms must fully enclose the work area. Wood debris, oils, and any other materials released into lake waters must be collected, removed, and properly disposed of at approved disposal sites.
  8. All in- and over-water wood cutting would be limited to the minimum required to remove the subject wood component, and all cutting work should be enclosed within floating containment booms.
  9. When removing piles, no actions shall be taken that would cause adhering sediments to return to lake waters.
  10. Above-water containment shall be installed around removed piles to prevent sediment laden waters from returning to lake waters.
  11. Construction staging (including stocking of materials, etc.) will occur on the supply barge.
  12. All Exposed wood to be used on the project will be treated with a cheminite treatment.



DETAIL 1.1 & 1.2



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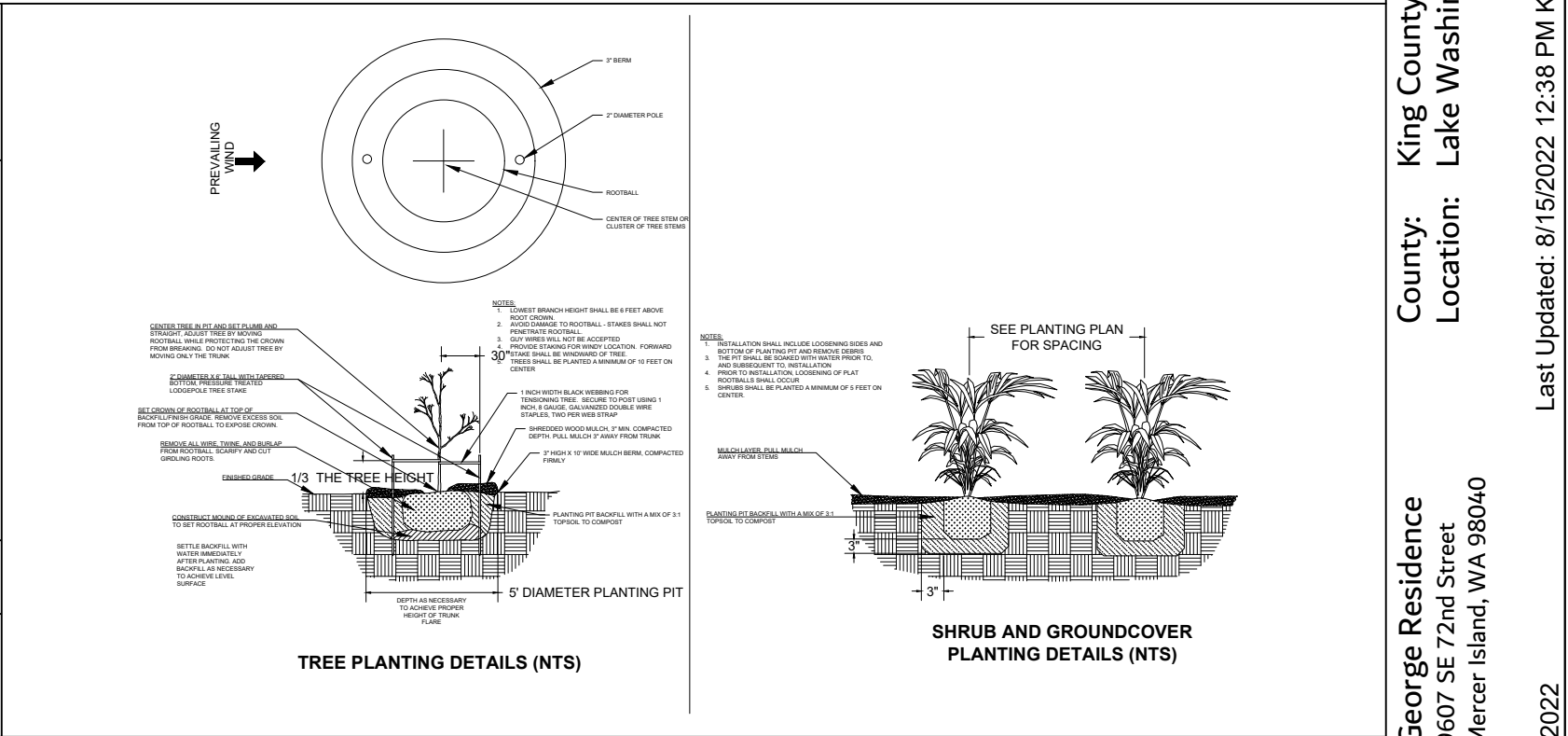
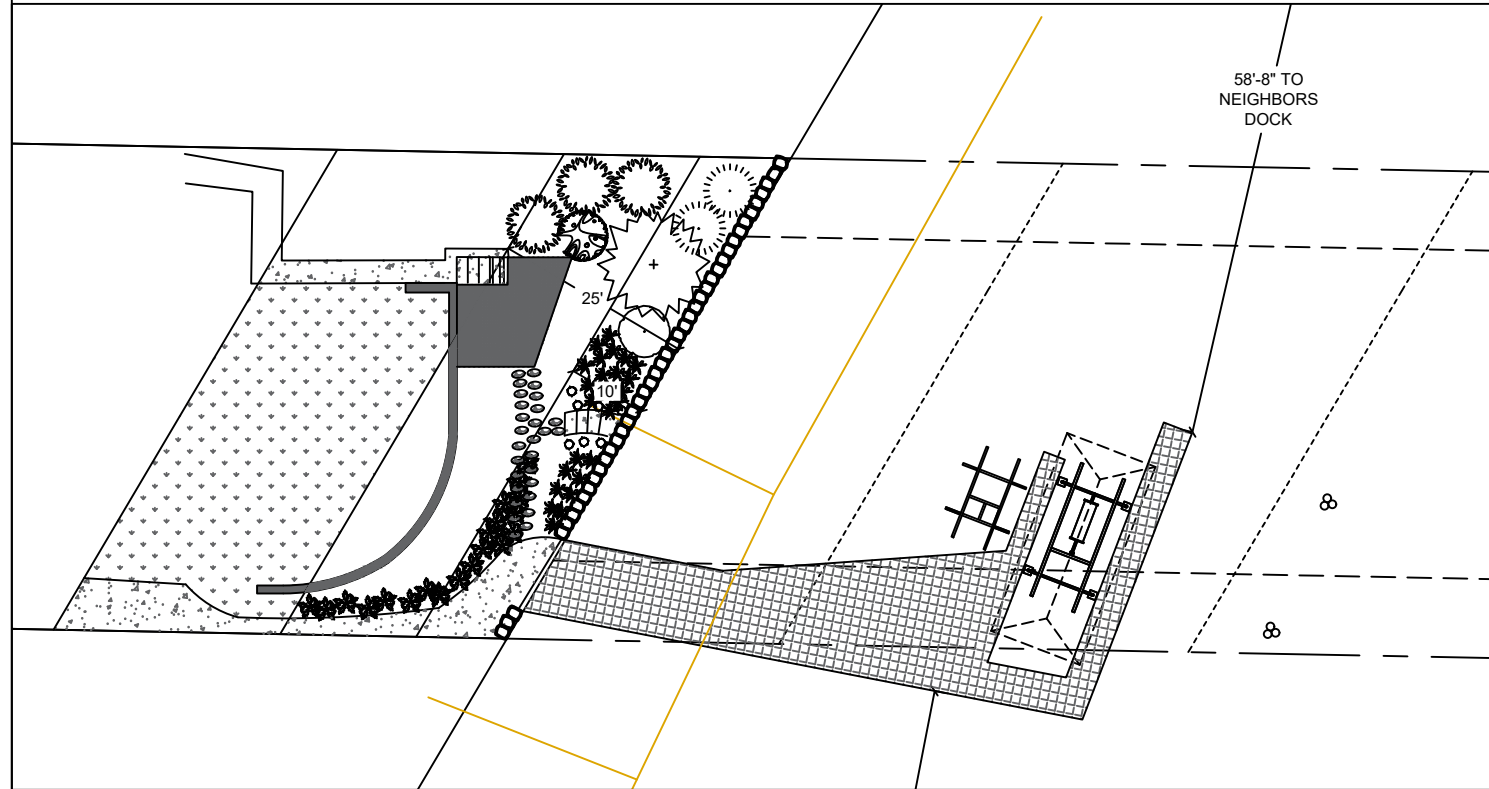
Adjacent Owners:  
BARTO M BRUCE+SANDRA S  
9601 SE 72ND ST 98040

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# MITIGATION PLAN



**Notes:**

1. Shrubs are show, and shall be planted, at least five feet on center. Trees are show, and shall be planted, at least ten feet to center.
2. The property owner will implement and abide by the shoreline planting plan. The plants shall be installed before or concurrent with the work authorized by this permit. A report, as-built drawing and photographs demonstrating the plants have been installed or a report on the status of project construction will be submitted to the U.S. Army Corps of Engineers, Seattle District, Regulatory Branch, within 12 months from the date of permit issuance. This reporting requirement may be met by completing and submitting a U.S. Army Corps of Engineers approved Report for Mitigation Work Completion form.
3. The property owner will maintain and monitor the survival of installed shoreline plantings for five years after the U.S. Army Corps of Engineers accepts the as-built report. Installed plants shall achieve 100% survival during monitoring Years 1 and 2. Installed plants shall achieve at least 80% survival during monitoring Years 3, 4 and 5. Percent survival is based on the total number of plants installed in accordance with the approved riparian planting plan. Individual plants that die will be replaced with native riparian species in order to meet the survival performance standards.
4. The property owner will provide annual monitoring reports for five years (Monitoring Years 1-5). Each annual monitoring report will include written and photographic documentation on plant mortality and replanting efforts and will document whether the performance standards are being met. Photos will be taken from established points and used repeatedly for each monitoring year. In addition to photos at designated points, photo documentation will include a panoramic view of the entire planting area. Submitted photos will be formatted on standard 8 1/2 x 11" paper, dated with the date the photo was taken, and clearly labeled with the direction from which the photo was taken. The photo location points will be identified on an appropriate drawing. Annual shoreline planting monitoring reports will be submitted to the U.S. Army Corps of Engineers, Seattle District, Regulatory Branch, by November 31 of each monitoring year. This reporting requirement may be met by completing and submitting a U.S. Army Corps of Engineers approved Mitigation Planting Monitoring Report form.

No additional plantings proposed due to extensive existing plantings.

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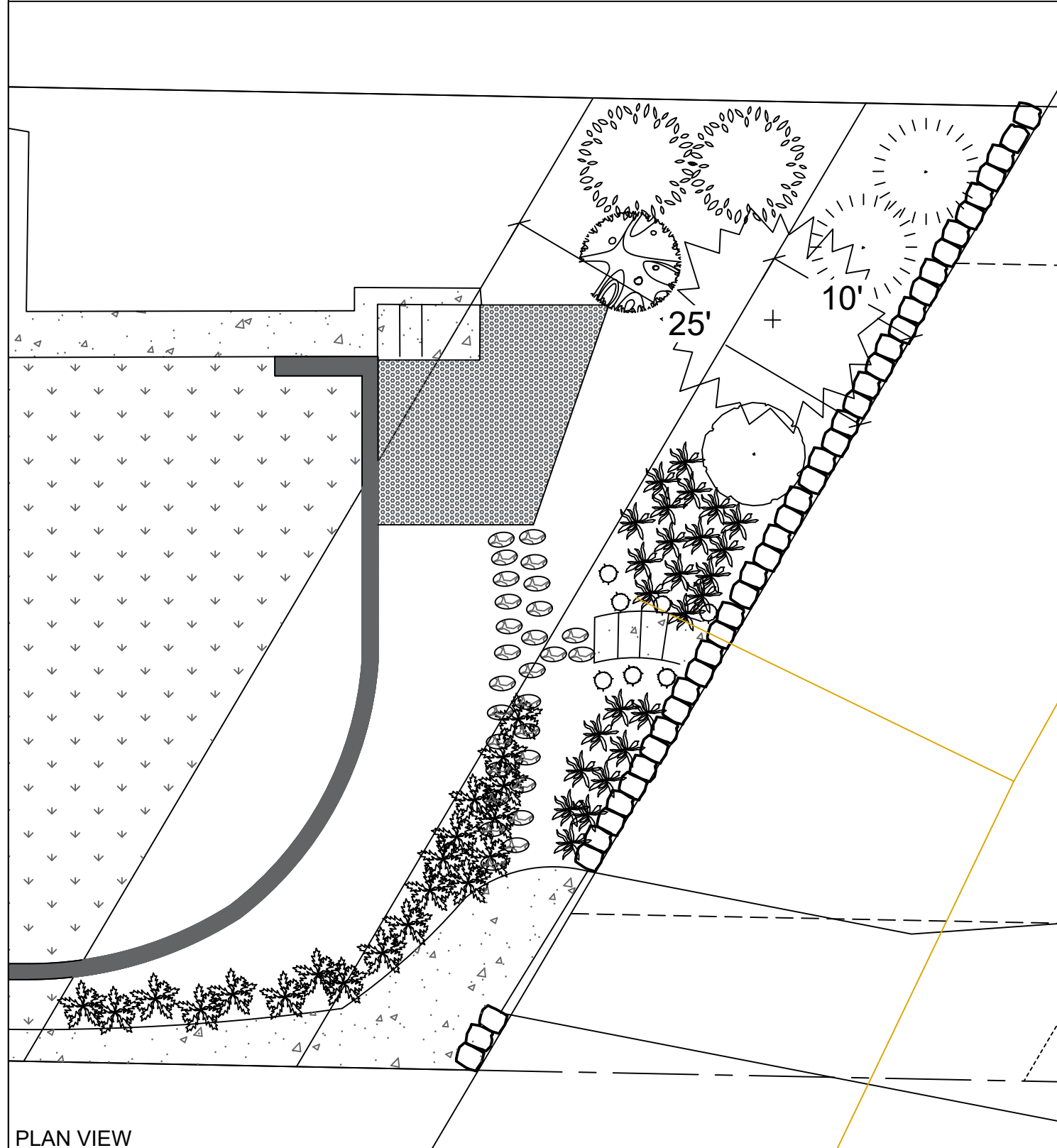


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**SHEET  
 A7.0**

# EXISTING PLANT PLAN



PLAN VIEW

EXISTING PLANTS TABLE

## EXISTING PLANTING SPECIES/QUANTITIES

SYMBOL	SCIENTIFIC NAME	COMMON NAME	QTY	SIZE
	<i>Sporobolus heterolepis</i>	Prairie dropseed	24	varies
	<i>Cornus sanguinea</i> 'Midwinter fire'	Bloodtwig dogwood	1	~10ft
	<i>Festuca Glauca</i>	Blue Fescue	22	~2ft
	<i>Lavandula angustifolia</i>	English lavender	6	~1ft
	<i>Hydrangea macrophylla</i>	Bigleaf hydrangea	2	~8ft
	<i>Juniperus Communis</i>	Common Juniper	3	~4ftx2ftx1ft
	<i>Acer palmatum</i>	Japanese Maple	1	~15ft
	<i>Hydrangea quercifolia</i>	Oakleaf hydrangea	1	~3ftx8ft



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**A8.0**



# GENERAL NOTES:

MATERIALS SPEC LIST:

**Boat Lifts:**

- \* SL10014ARW - 146" x 191"

**Decking Material:** FRPP - Fiberglass reinforced polypropylene

Light permeable percentage:

- \* Surface - 43%
- \* 18" Dock Height - 61%

**SEWER:**

- \* All sewer is field verified by probing the lake bed manually during the allowed work windows for the area.

**PILES:**

- \* Repair piles are done as a sleeve/strap method

CODE REFERENCES: Mercer Island

We are applying for the permit to be reviewed under the:  
 "Development Standards for Replacement, Repair and Maintenance of Overwater Structures, Including Moorage Facilities" per MIMC 19.13050(F)(2).

The maintenance, repair and complete replacement of legally existing overwater structures is permitted; provided, that:

- i) All permit requirements of federal and state agencies are met;
  - **WDFW & CORPS permits are being applied for**
- ii) The area, width, or length of the structure is not increased, but may be decreased;
  - **Repair only, we are not altering the existing structure in size**
- iii) The height of any structure is not increased, but may be decreased; provided, that the height above the OHWM may be increased as provided in subsection (F)(2)(ix)(b) of this section;
  - **We are not increasing the height of the structure**
- iv) The location of any structure is not changed unless the applicant demonstrates to the director's satisfaction that the proposed change in location results in: (A) a net gain in ecological function, and (B) a higher degree of conformity with the location standards for a new overwater structure;
  - **No structure is being relocated**
- v) Piles shall not be treated with pentachlorophenol, creosote, CCA or comparably toxic compounds. If ammoniacal copper zinc arsenate (ACZA) pilings are proposed, the applicant shall meet all of the best management practices, including a post-treatment procedure, as outlined in the amended Best Management Practices of the Western Wood Preservers. All piling sizes are in nominal diameter;
  - **Piles will meet BMP, no new piles on this project**
- vi) Any paint, stain or preservative applied to components of the overwater structure must be leach resistant, completely dried or cured prior to installation. Materials shall not be treated with pentochlorophenol, creosote, CCA or comparably toxic compounds;
  - **Construction follows BMP**
- vii) The applicant shall abide by the work windows for listed species established by the U.S. Army Corps of Engineers and Washington Fish and Wildlife;
  - **We will follow the work windows for the CORPS and WDFW**
- viii) Disturbance of bank vegetation shall be limited to the minimum amount necessary to accomplish the project. Disturbed bank vegetation shall be replaced with native, locally adapted herbaceous and/or woody vegetation. Herbaceous plantings shall occur within 48 hours of the completion of construction. Woody vegetation components shall be planted in the fall or early winter, whichever occurs first. The applicant shall take appropriate measures to ensure revegetation success; Ch. 19.13 Shoreline Master Program | Mercer Island City Code Page 29 of 34 The Mercer Island City Code is current through Ordinance 20C-13, passed June 16, 2020.
  - **Understood**
- ix) Structural Repair. The structural repair, which may include replacement of framing elements, of moorage facilities that results in the repair of more than 50 percent of the structure's framing elements within a five-year period shall comply with subsections (F)(2)(ix)(a) through (F)(2)(ix)(c) of this section. For this section, framing elements include, but are not limited to, stringers, piles, pile caps, and attachment brackets, as shown in Figure D:
  - a) One hundred percent of the decking area of the pier, dock, and any platform lifts must be fully grated with materials that allow a minimum of 40 percent light transmittance;
    - **We are not repairing over 50% but we will make sure the decking meets the light requirements**
  - b) The height above the OHWM for moorage facilities, except floats, shall be a minimum of one and one-half feet and a maximum of five feet; and
    - **We are not repairing over 50% and thus not able to adjust the dock height**
  - c) An existing moorage facility that is five feet wide or more within 30 feet waterward from the OHWM shall be replaced or repaired with a moorage facility that complies with the width of moorage facilities standards specified in Table D of this section;
    - **We are not repairing over 50% and thus this section does not apply**
  - x) Exterior Surface Repair. The exterior surface repair, which may include the replacement of exterior surface materials of moorage facilities that results in the repair of more than 50 percent of the surface area of the moorage facility's decking, fascia, and platform lifts within a five-year period (see Figure D), shall be required to utilize materials that allow a minimum of 40 percent light transmittance over 100 percent of the dock; and
    - **We are not repairing over 50% and thus this section does not apply**
  - xi) Any decking that is removed in the course of repair shall be replaced with decking materials that allow a minimum of 40 percent light transmittance.
    - **We are not repairing over 50% but we will make sure the decking meets the light requirements**

Last permit issued for property: SHL9607-140 (1996)  
 Dock established/constructed: 92-804 (Repair permit 10-15-1996)  
 \* PWC Lift & boatlift w/canopy on included on repair permit  
 Hardscape existing: 1307-168 (2014)

<p><b>County:</b> King County</p> <p><b>Location:</b> Lake Washington</p>	<p><b>Applicant:</b> George Residence</p> <p>9607 SE 72nd Street                  Mercer Island, WA 98040</p>	<p><b>Datum:</b> CORPS OF ENGINEERS 1919</p> <p>NE Quarter Of Section 30, Township 24, Range 05</p> <p><b>Adjacent Owners:</b></p> <p>BARTO M BRUCE+SANDRA S                  9601 SE 72ND ST 98040</p> <p>GARTON KYLE JHRACHEL A                  9615 SE 71ST ST 98040</p>
<p><b>SHEET A9.0</b></p>		
<p>NWS-2021-XXX                  PAGE 9 OF 14</p>		



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Last Updated: 8/15/2022 12:38 PM Kelse

Created: 1-24-2022

# HARDSCAPE CALCULATION

## Allowed Hardscape Quantities

SETBACK (total sqft)	ALLOWED	PERMITTED 4/4/2014	CURRENTLY USED	NEED TO REMOVE	PROPOSED
0' - 25' SETBACK (1,765.7547 sqft)	10% (176.5754 sqft)	19.5183% (344.6462 sqft)	35.2472% (622.3785 sqft)	15.7288% (277.7323 sqft)	19.5183% PERMITTED (344.6462 sqft)
25' - 50' SETBACK (1,765.7547 sqft)	30% (529.7264 sqft)	7.635% (130.0215 sqft)	14.0133% (247.4415 sqft)	0% (0 sqft)	14.0133% (247.4415 sqft)

## LEGEND - EXISTING 25' SETBACK

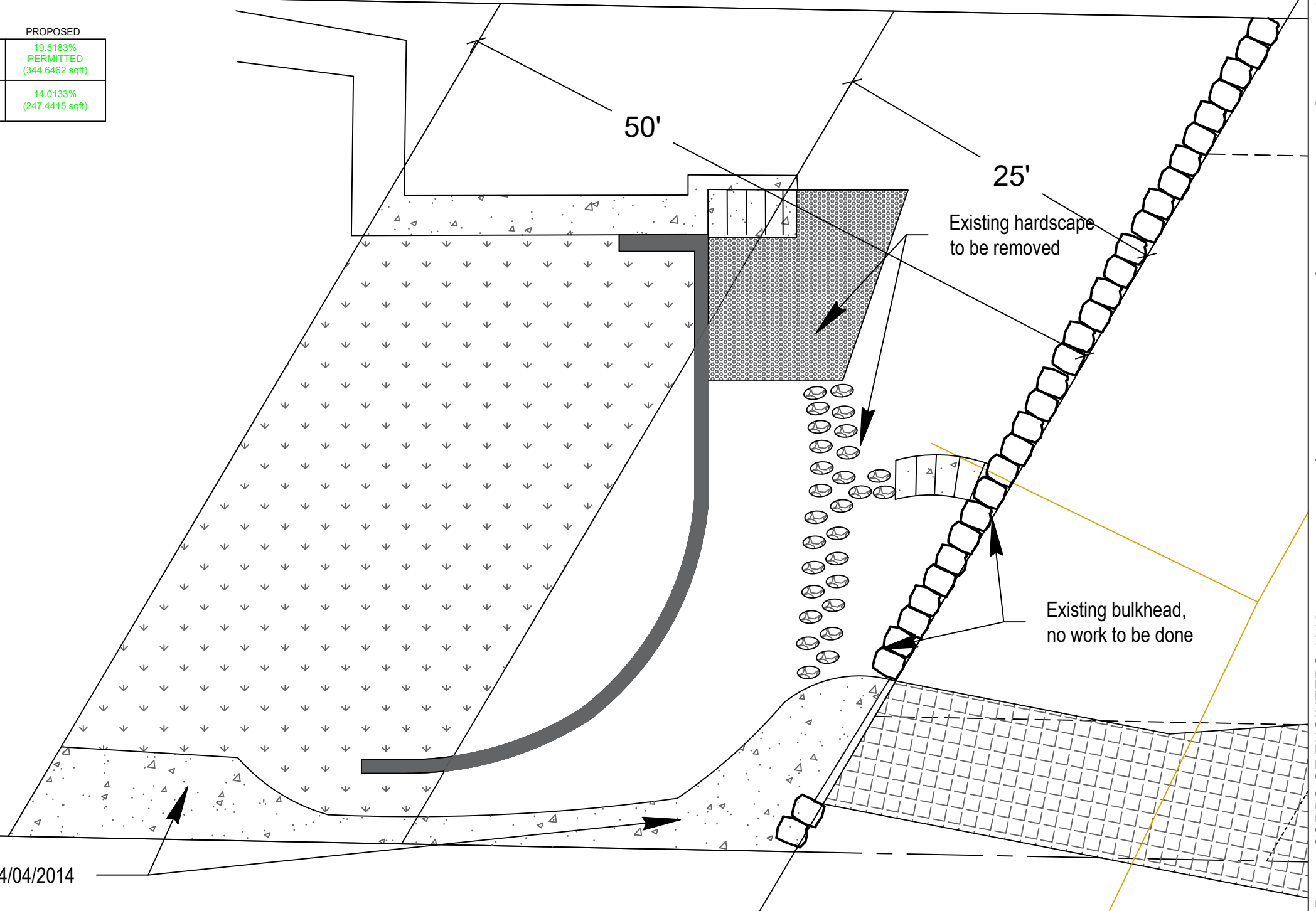
DESCRIPTION	AREA	HATCH
RETAINING WALL 25FT SETBACK	40.972 sqft	[Hatch: Bricks]
PATIO - GRAVEL 25FT SETBACK	145.7603 sqft	[Hatch: Gravel]
28 STONE STEP PADS 25FT SETBACK	91 sqft (3.25 each)	[Hatch: Stone]
57 L.F BULKHEAD (PERMITTED) 25FT SETBACK	171 sqft (3' W BULKHEAD)	[Hatch: Bulkhead]
CONCRETE STAIRS (PERMITTED) 25FT SETBACK	18.441 sqft	[Hatch: Concrete]
CONCRETE PATH (PERMITTED) 25FT SETBACK	155.2052 sqft	[Hatch: Concrete]

## LEGEND - EXISTING 50' SETBACK

DESCRIPTION	AREA	HATCH
RETAINING WALL 50FT SETBACK	20.55 sqft	[Hatch: Bricks]
CONC. PATH/STAIRS (N. side) 50FT SETBACK	96.87 sqft	[Hatch: Concrete]
CONCRETE PATH (PERMITTED) 50FT SETBACK	130.0215 sqft	[Hatch: Concrete]

## LEGEND - MATERIAL UPDATE 25' SETBACK

DESCRIPTION	AREA	HATCH
PATIO - GRAVEL TO WOOD CHIPS 25FT SETBACK	145.7603 sqft	[Hatch: Gravel]
28 STONE STEP PADS TO WOOD CHIPS 25FT SETBACK	28 sqft	[Hatch: Stone]
57 L.F BULKHEAD (PERMITTED) 25FT SETBACK	114 sqft (2' W BULKHEAD)	[Hatch: Bulkhead]



PATH PERMITTED 04/04/2014



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CORPS OF ENGINEERS 1919  
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NE Quarter Of Section 30, Township 24, Range 05

Adjacent Owners:  
BARTO M BRUCE+SANDRA S  
9601 SE 72ND ST 98040  
GARTON KYLE J+RACHEL A  
9615 SE 71ST ST 98040

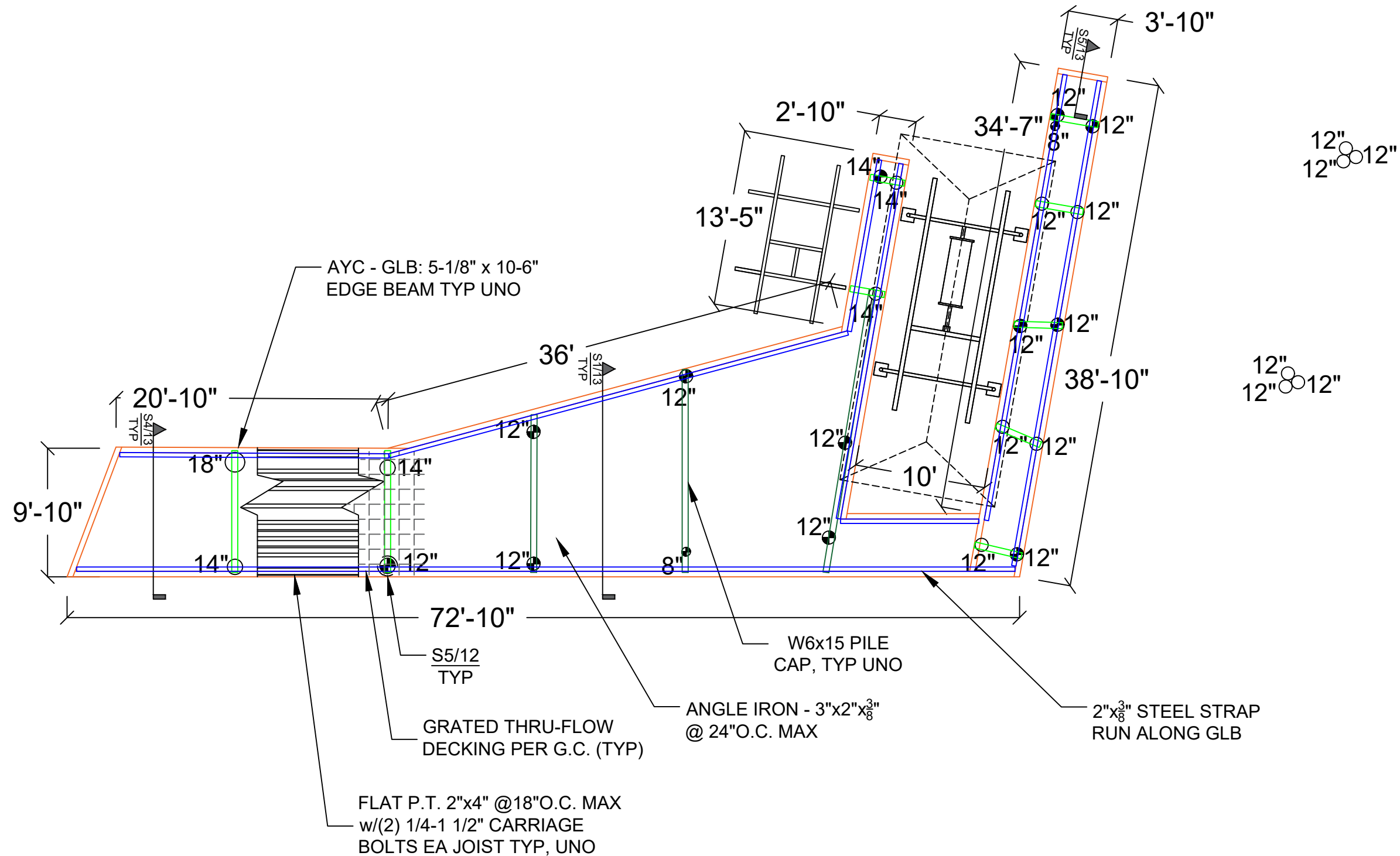
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Last Updated: 8/15/2022 12:38 PM Kelse

Created: 1-24-2022

# FRAMING LAYOUT



TAKEOFF LAYOUT



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1080 W Ewing St  
Seattle, WA 98119

Office: 206-236-1700 ext. 3  
www.seabornpiledriving.com

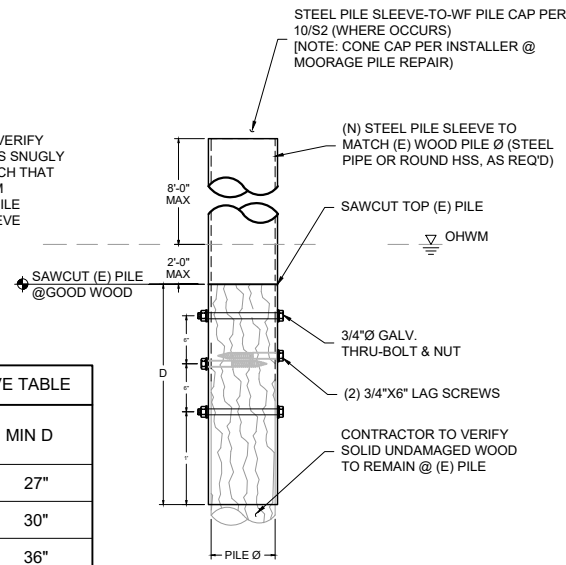
Scope of Work: We propose to repair (14) dock piles, repair the 57 linear feet of existing rock bulkhead, remove approximately 214 sqft of hardscape, remove (6) dock piles & repair the existing decking with grated material.

County: King County  
Location: Lake Washington  
Applicant: George Residence  
9607 SE 72nd Street  
Mercer Island, WA 98040  
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NWS-2021-XXX  
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# DETAILS - REPAIR

CONTRACTOR TO VERIFY THAT THE PILE FITS SNUGLY IN THE SLEEVE SUCH THAT THERE IS UNIFORM BEARING OF THE PILE AGAINST THE SLEEVE



STEEL SLEEVE TABLE	
PILE Ø	MIN D
8"	27"
10"	30"
12"+	36"

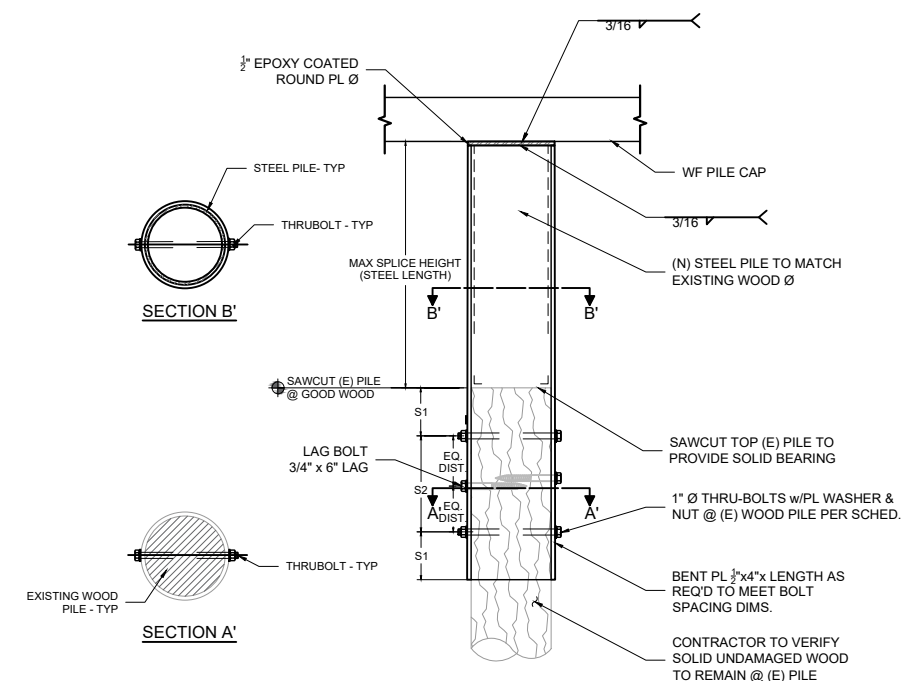
**S1** SLEEVE PILE REPAIR - TYP  
SCALE: 1/2" = 1'

**S2** DETAIL NOT BEING USED

**S3** DETAIL NOT BEING USED

STEEL SLEEVE TABLE				
PILE Ø	MAX SPLICE HEIGHT	S1	S2	S3
9"	15"	6"	12"	3"
12"+	28"	6"	12"	3"

- NOTES:
- 1) MAX PILE LOAD = 1,200 # / PILE (BOAT LOAD PARALLEL TO DOCK). CONTACT ENGINEERING IF BOAT SIZE EXCEEDS GENERAL NOTES MAXIMUM ALLOWABLE.
  - 2) MINIMUM BOLT SPACING = 3"
  - 3) STRAP AXIS SHALL BE ORIENTED PERPENDICULAR TO LONGITUDINAL AXIS OF DOCK.



**S4** DETAIL NOT BEING USED

**S5** SPLICE PILE REPAIR - TYP  
SCALE: 1/2" = 1'



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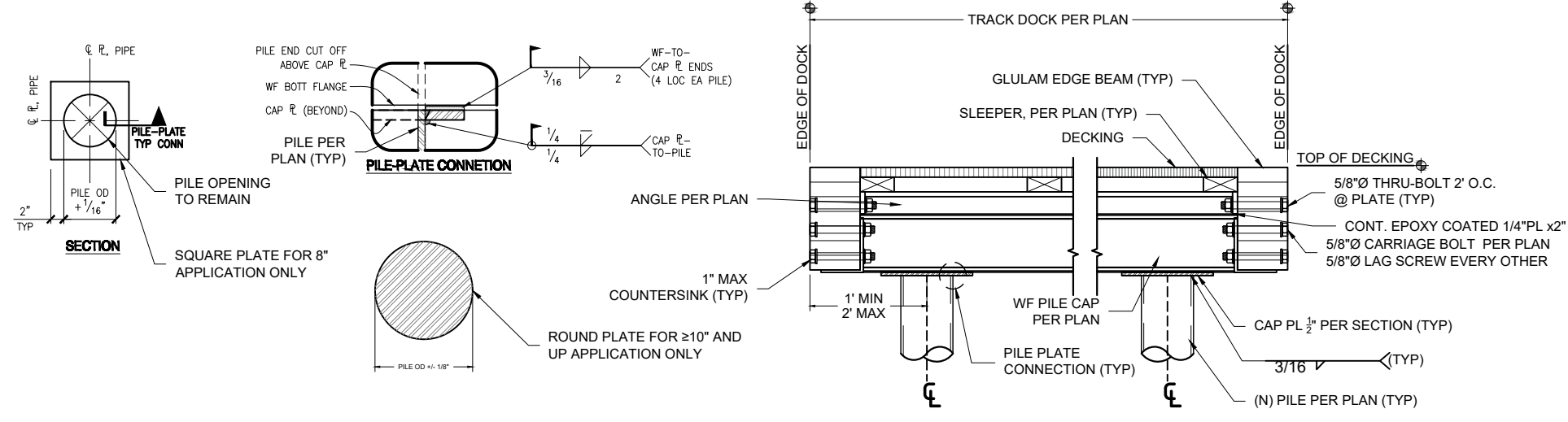
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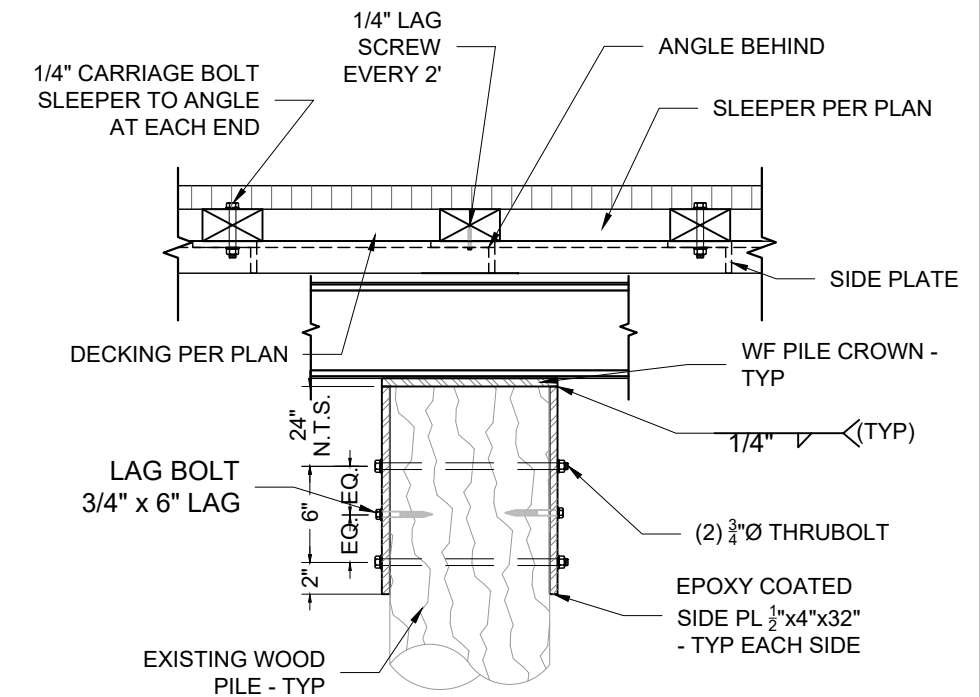
Created: 1-24-2022

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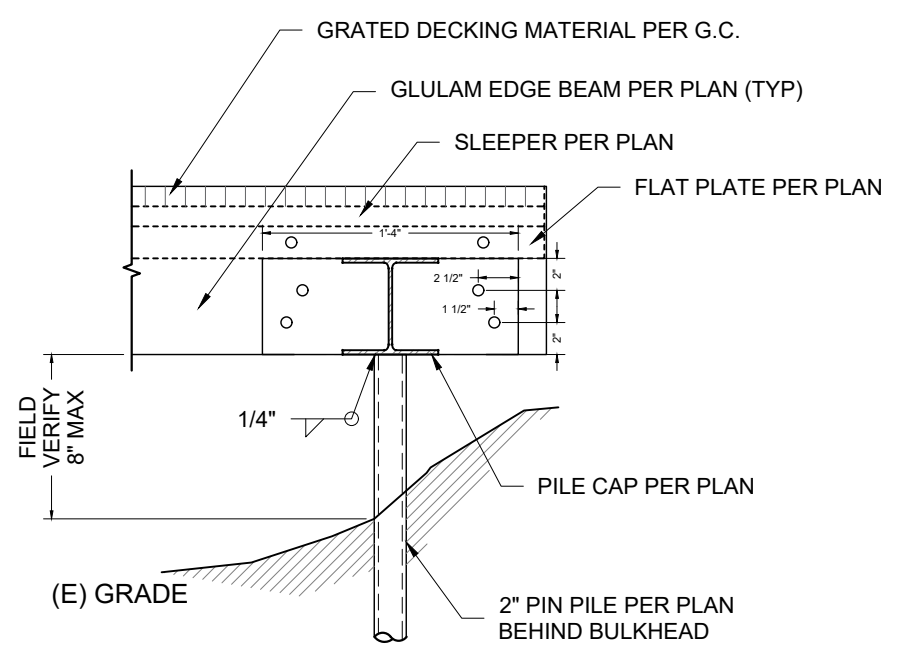
# DETAILS - REPAIR



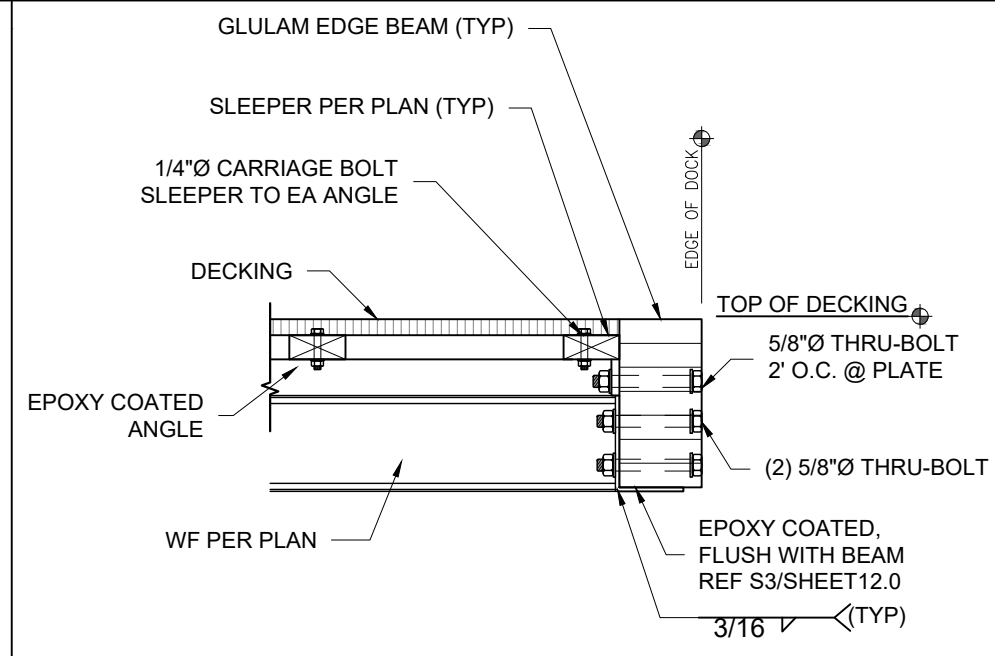
**S1 DOCK SECTION**  
SCALE: 3/4" = 1"



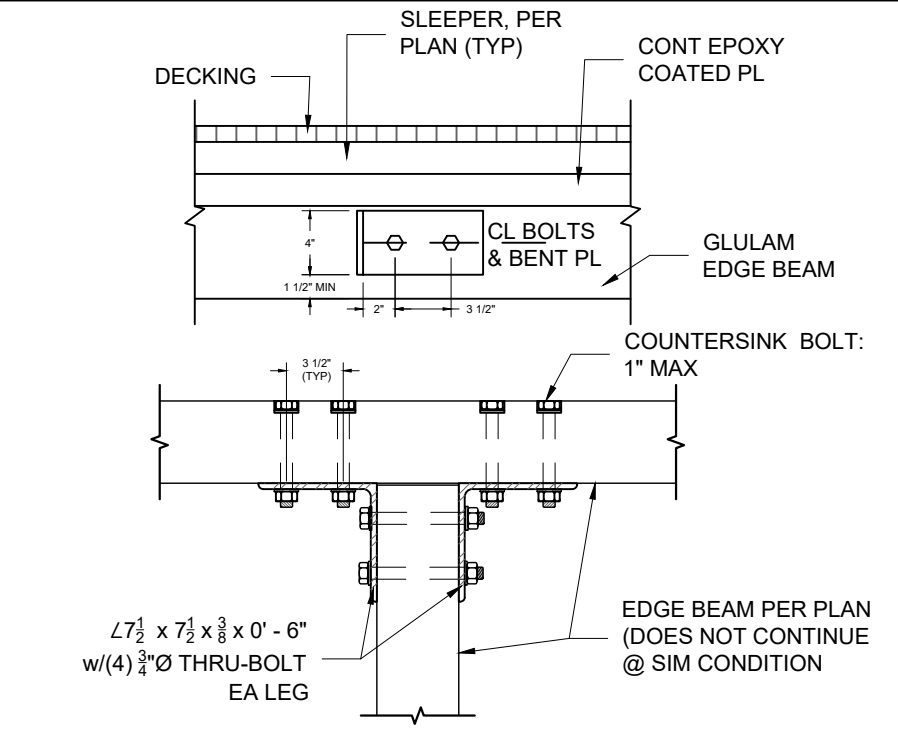
**S3 I-BEAM TO WOOD PILE - TYP**  
SCALE: 1" = 1"



**S4 PIN PILE @ SHORE MOUNT - TYP**  
SCALE: 1" = 1"



**S5 EDGE SECTION (STEEL TRACK) - TYP**  
SCALE: 1" = 1"



**S6 BEAM TO BEAM - TYP**  
SCALE: 1" = 1"



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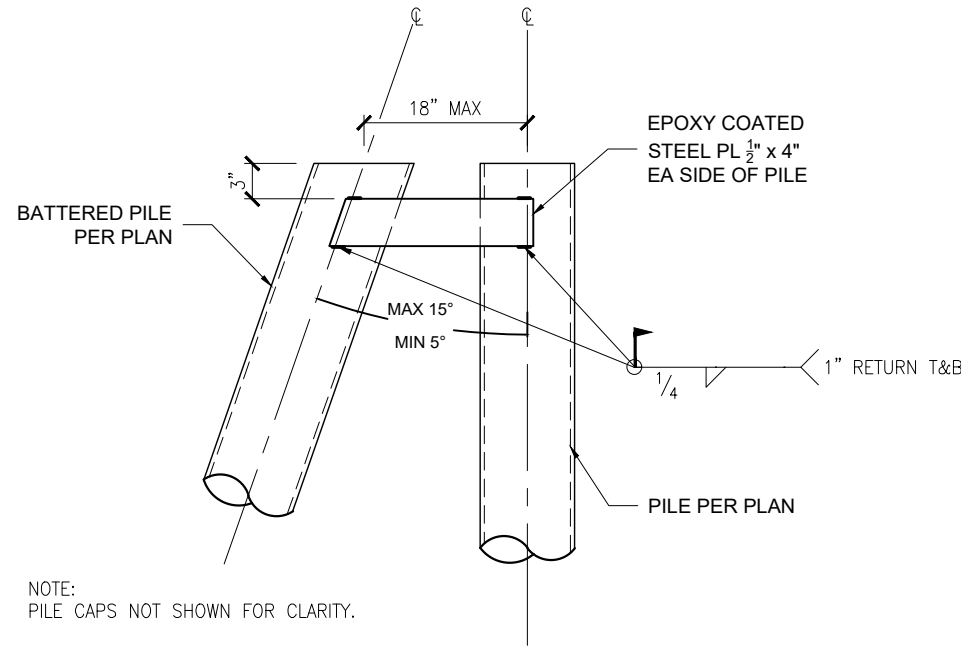
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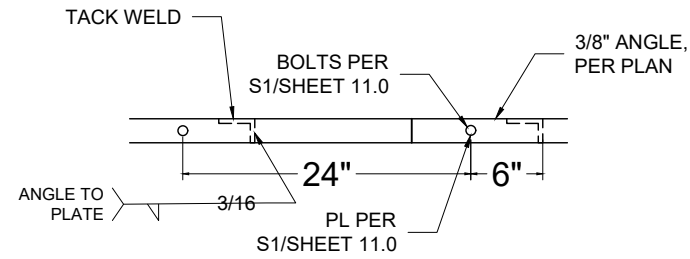
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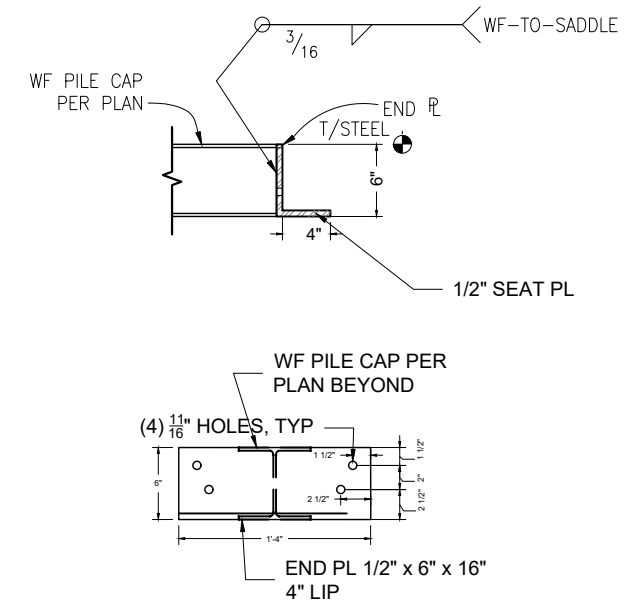
# DETAILS - REPAIR



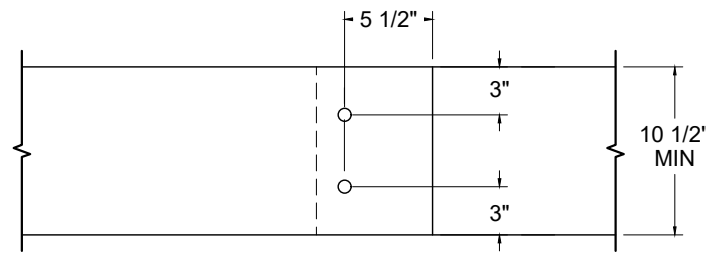
**S1** BRACE PILES - TYP  
SCALE: 3/4" - 1"



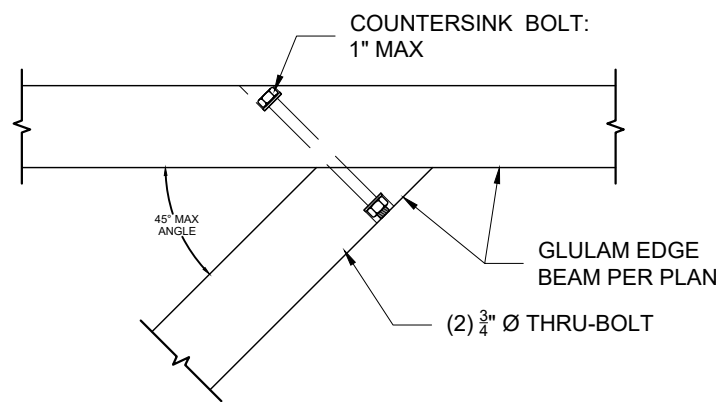
**S2** ANGLE TO PLATE - TYP  
SCALE: 3/4" - 1"



**S3** BEAM SADDLE BRACKET  
SCALE: 3/4" - 1"



**S4** FILET CONNECTION - TYP  
SCALE: 1" - 1"



**S5** DETAIL NOT BEING USED  
SCALE: 1" - 1"

**S6** DETAIL NOT BEING USED  
SCALE: 1" - 1"



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**SHEET**  
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